

# The City of Kapolei

## URBAN DESIGN PLAN

November 2007



# The City of Kapolei

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## URBAN DESIGN PLAN

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Honolulu, Hawai'i

November 2007

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The City of Kapolei  
URBAN DESIGN PLAN

1.0 Introduction



## 1.0 INTRODUCTION TO THE PLAN

### 1.1 PURPOSE AND INTENT

The City of Kapolei Urban Design Plan (UDP) defines the City's design character, and sets forth concepts, standards and guidelines for its development as the urban core of the greater Kapolei region. The document is to be used by prospective developers, the State of Hawai'i (State), the City & County of Honolulu (City & County), James Campbell Company LLC (Campbell Companies) and the Kapolei Design Advisory Board (DAB) in the development and review of projects. For the purposes of this document, Campbell Companies includes Kapolei Property Development LLC, Aina Nui Corporation and other affiliates.

The development concepts, standards and guidelines contained in the UDP are general in nature and provide an overall design framework to create compatible developments within the general fabric of the City of Kapolei.

**Concepts** are regional in scope and include overarching notions such as the "Hawaiian Garden City" and "connectivity". These are to guide the development of the City of Kapolei and are presented in statements generally using the word "should".

**Standards** are requirements that must be addressed to achieve the intent of the concepts. Examples of standards are: kama'āina architecture building styles; landscape treatments and species; and building orientation and setbacks. Standards are presented in statements using the words "shall" or "will".

**Guidelines** provide an appropriate range of choices to implement the standards. For example, within the prescriptive architectural character standard, there are various ways to achieve the standard, such as massing, materials and colors. Guidelines are presented in statements using the words "should" or "encouraged".

The UDP supplements and is subject to existing regulatory controls, including the zoning and subdivision requirements adopted by the City & County. Should any conflict arise between City & County regulations and the urban design standards established in this document, the more restrictive provisions shall prevail.

The City Council of Honolulu (City Council), as part of the conditions upon which zoning approval for City of Kapolei was granted, first approved the UDP in 1991. Subsequent to this, the City Council has approved amendments to this UDP through Council resolutions.

Individual development projects are subject to the review and approval of a DAB established by Campbell Companies. The City and County Department of Planning and Permitting (DPP) also reviews plans to assure compliance with this UDP. Project approval is required prior to building permit application and commencement of construction. Project submittal requirements and review procedures for the DAB are presented in Chapter 6.0.

Urban Design Plan compliance is a condition of zoning for properties in and around the City of Kapolei. For a subset of these properties annexed within the City of Kapolei, UDP compliance is also mandated by a declaration of protective covenants, conditions and restrictions.

## CHAPTER 1: Introduction

### 1.1 Purpose and Intent

# CHAPTER 1: Introduction

## 1.2 Organization



## 1.2 ORGANIZATION

The UDP addresses the overall mix of land uses for the City of Kapolei and the City's design character and development quality. The UDP is organized into the following chapters:

- Chapter 2.0 Development Framework
- Chapter 3.0 Urban Concept and Themes
- Chapter 4.0 Supporting Themes and Unifying Elements
- Chapter 5.0 Districts
- Chapter 6.0 Project Design Review Procedures
- Chapter 7.0 Amendment Procedures

The City of Kapolei  
URBAN DESIGN PLAN

2.0 Development Framework

## 2.0 DEVELOPMENT FRAMEWORK

### 2.1 LOCATION AND PHYSICAL SETTING

The UDP covers the triangularly-shaped 650± acre City of Kapolei bounded by Fort Barrette Road (also known as Barbers Point Access Road) on the east, Kalaeloa (formerly Barbers Point Naval Air Station) and the OR&L right-of-way on the south (makai), Kalaeloa Boulevard on the west, and the H-1 Freeway corridor on the north (mauka) (Figures 1 and 2).

The City is located on the 'Ewa Plain and is set at the foot of the Wai'anae Mountain Range. Pu'u Pālailai (height 492 feet) lies about one-half mile mauka, while Pu'u Kapolei (height 166 feet) anchors the east end of the City and is its most important physiographic feature. Except for Pu'u Kapolei, the land is very gently sloping. Cultivated with sugar cane for many decades, the area is now rapidly developing.

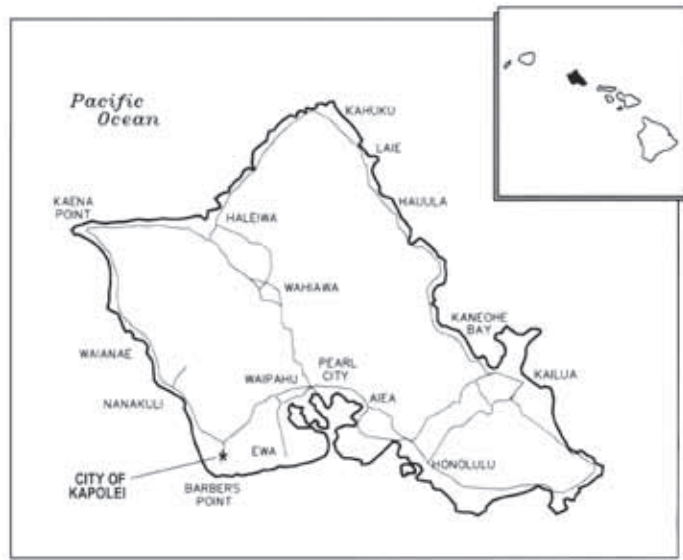


FIGURE 1 VICINITY MAP

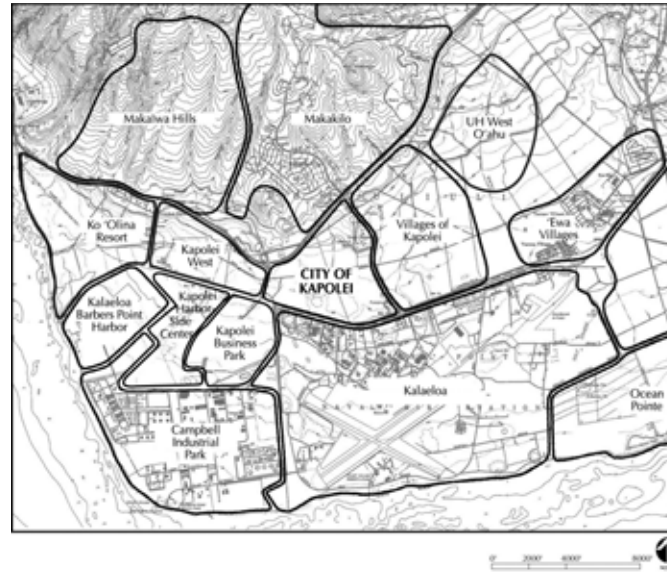


FIGURE 2 CITY LOCATION

### 2.2 MASTER PLAN HISTORY

The first 'Ewa Master Plan was prepared for the Estate of James Campbell by Harland Bartholomew and Associates in 1955. By 1974 it had been revised twice by Donald Wolbrink and Associates. All three plans envisioned a major new urban community on the 'Ewa Plain.

In the early 1970's, the City & County Planning Department began a "General Plan Revision Program" which focused on directing growth to 'Ewa. This culminated with the adoption of a new O'ahu General Plan in 1977 and designated City of Kapolei as a "Secondary Urban Center" (SUC) in order to "...relieve developmental pressures in the urban fringe and rural areas".

## CHAPTER 2: Development Framework

### 2.1 Location and Physical Setting

### 2.2 Master Plan History

## CHAPTER 2: Development Framework

### 2.3 City of Kapolei and Surrounding Area Long Range Master Plan

In the early 1980's, the 'Ewa Master Plan was revised to identify the location of a major "City Center" between Makakilo, Campbell Industrial Park and the Barbers Point Naval Air Station. Amendments to the 1986 City & County's 'Ewa Development Plan established the urban nucleus of the SUC at its current location. Later in 1986, the Estate completed a detailed implementation plan for the City Center. At this time, the 'Ewa Master Plan became the landowners' Kapolei Area Long Range Master Plan in Figure 3.

#### 2.3 CITY OF KAPOLEI AND SURROUNDING AREA LONG RANGE MASTER PLAN

The Kapolei region is developing as a balanced area with a full range of housing, jobs, and public facilities consistent with a true urban area. An essential part of the Master Plan is the development of a city center, the City of Kapolei. Higher density land uses such as retail, office, public facilities, and other urban-type land uses are concentrated in the City, providing employment for West O'ahu's growing population.

As of 2006, more than 800 companies, agencies, and organizations account for nearly 25,000 jobs in the region. Recent studies indicate that an additional 40,000 jobs will be created by 2025 as the City evolves.

The former Barbers Point Naval Air Station, which closed in 1999 is directly makai of the city. The land is under the jurisdiction of the State Hawai'i Community Development Authority and is known as Kalaeloa.

James Campbell Industrial Park is southwest of the City of Kapolei. Its 1,380 acres are completely developed, sold or leased. Located between the Industrial Park and the City is the 140-acre Kapolei Business Park. It is also rapidly developing.

Kalaeloa Barbers Point Harbor is located on the coast to the west of the Industrial Park. It is the State of Hawai'i's second busiest commercial port after Honolulu Harbor. Kalaeloa Barbers Point Harbor handles primarily bulk cargo. East of the harbor are 70 acres of land planned for the Kapolei Maritime Industrial Park, and an additional 350 acres of land planned for general industrial use known as Kapolei Harborside.

Just north of Kalaeloa Barbers Point Harbor lies Ko 'Olina, a 620 acre master-planned destination resort and residential community. Ko 'Olina includes over 1,100 visitor units, either built or in design, 1,100 housing units, either built or in design, 330-slip marina, a championship 18-hole golf course, and four sandy lagoons. Total build-out of Ko 'Olina is projected to include over 9,000 visitor and housing units, plus additional amenities.

The new campus for the University of Hawai'i West O'ahu is planned along the future North-South Road just south of Farrington Highway.

Existing residential communities in the region include 'Ewa Beach, 'Ewa by Gentry, Ocean Pointe, 'Ewa Villages, Honokai Hale/Nanakai Gardens, Makakilo, Villages of Kapolei, Kapolei Knolls, and West Loch Estates. As of 2006, the number of homes in the region is estimated at 26,000. Proposed projects which are in various stages of planning and approvals include Kapolei West, Maka'iwa Hills and East Kapolei. Expansion and infill of communities and the construction of new residential projects, will add an additional 25,000 residential units to the area by 2025.

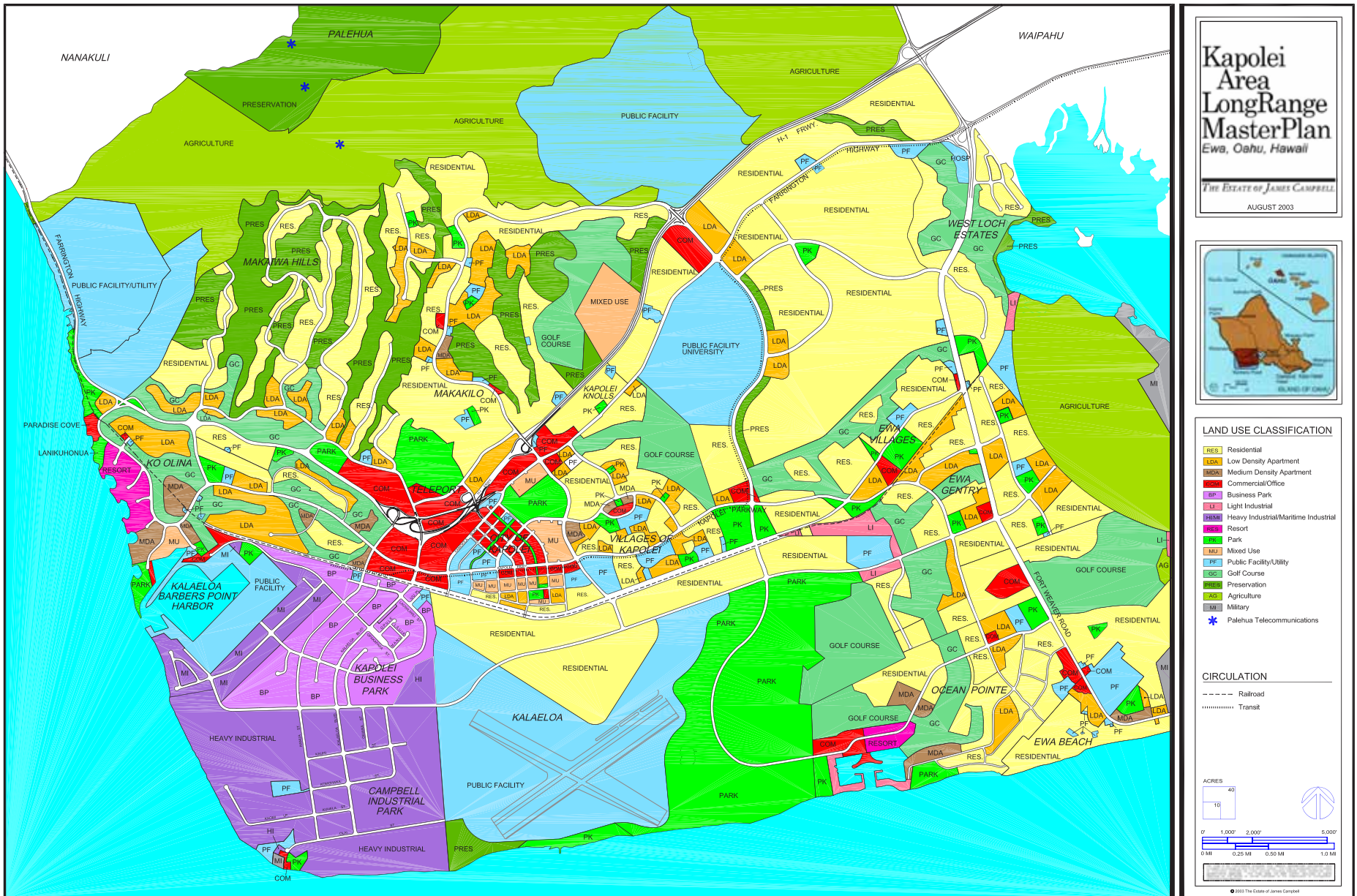


FIGURE 3

CITY OF KAPOLEI AND SURROUNDING AREA LONG RANGE MASTER PLAN (2003)

## CHAPTER 2: Development Framework

### 2.4 Zoning

### 2.5 Growth Projections

## 2.4 ZONING

Historically, most of the 'Ewa Region was zoned Agriculture reflecting its past use and the historical use of this zoning as a default designation for open land. The entire 650-acre City of Kapolei is zoned to reflect commercial, mixed use, and residential uses in accordance with the Master Plan (Figure 3) and the City's 'Ewa Development Plan. The adoption of urban zoning is continuing for surrounding areas consistent with these plans.

## 2.5 GROWTH PROJECTIONS

DPP population projections and other studies indicate the continuation of significant growth. Population within the Kapolei region (DPP 'Ewa Development Plan area) is expected to grow from 67,000 in 2000 to between 170,000 and 185,000 by 2025. The region's share of O'ahu's population is expected to increase to 16.5% during the same period.

The Kapolei region's commercial, industrial and residential development will create a significant increase in the number of jobs available in Kapolei. DPP economic projections and other recent studies indicate that jobs within the area shown on the Master Plan will increase by over 200% from 19,000 in 2000 to 65,000 by 2025. As of 2005, the City of Kapolei provided 25% of the region's jobs. By 2025, jobs in the City of Kapolei are projected to total over 22,000, or 34% of the regional total.



*Pālailai Mall between Campbell Square and Bank of Hawaii*

The City of Kapolei  
URBAN DESIGN PLAN

3.0 Urban Concept and Themes



## 3.0 URBAN CONCEPT AND THEMES

### 3.1 DEVELOPMENT OBJECTIVES

Development objectives for the City of Kapolei include provisions for:

- An employment center that establishes a balance between jobs and housing in the region, thus minimizing commutes to and from downtown Honolulu.
- A new center for offices and businesses which serve Leeward O'ahu but are currently in downtown Honolulu or other areas outside of the Kapolei region.
- A center for government offices, civic and community services, and recreational and cultural amenities.
- A city of people on bicycles, buses or foot where it is convenient and safe to travel to work, run errands or engage in recreation.
- A city where energy efficient practices such as recycling and water conservation techniques are firmly established.
- A city with street connectivity that makes travel more direct, shortens trips and provides a wide variety of travel choices.

### 3.2 DEVELOPMENT OF UDP

The preparation of the UDP in 1990 involved a five-level design process. It began with defining the desired overall city character and moved through progressively greater levels of detail to relatively specific development guidelines. These five levels are: design concept, major themes, composition, supporting themes, and unifying themes. The results of the design process continue to guide the plan through subsequent updates.

The overall concept and major themes for the City are described below and illustrated in Figure 4. Composition, supporting themes and unifying elements are discussed in Chapters 4.0 and 5.0.

### 3.3 DESIGN CONCEPT

The design concept envisions and captures a community's lifestyle; it describes how people will live, work and play in the city. For the City of Kapolei, it involves three lifestyle elements:

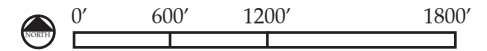
- City - diversity of uses within easy walking distance of each other, relatively high intensities of activity, and ease in bringing people together for both business and social meetings in a variety of public and private places.
- Healthy Environment - emphasis on the outdoors with ample attractive environments in which to enjoy a variety of passive and active recreational activities.
- Hawaiian Setting - urban density in harmony with the natural environment, a combination of the best urban activities and amenities of Honolulu in the larger regional context of a more relaxed neighbor island lifestyle.

## CHAPTER 3: Urban Concept and Themes

### 3.1 Development Objectives

### 3.2 Development of UDP

### 3.3 Design Concept



CITY OF KAPOLEI URBAN DESIGN CONCEPTUAL PLAN

FIGURE 4

### 3.4 MAJOR THEMES

Seven themes support the lifestyle concept, physical form and organization of the City of Kapolei's development:

- Hawaiian Garden City - A city with a garden setting characterized by major parks and boulevards with trees, flowers and groundcover.



*Hawaiian Garden City*

- Healthy Living - The garden city is to encourage a healthy, outdoor and active lifestyle through a variety of recreational features and facilities.
- Complete Community Services - The theme of healthy living is to be supported by medical/health/fitness services and facilities. The City of Kapolei is also to serve the surrounding region with recreational, cultural and religious facilities, State and City & County government offices, and quality public and private schools.
- Pedestrian Friendly - Within the garden city, an attractive network of paths for walking and biking will allow for convenient access between homes, jobs and community services.

- Past/Present/Future - Building styles and forms are to reflect the more intimate human scale of Hawai'i's older commercial districts, while recognizing present needs and future market forces which shape city environments.
- Sustainability - Resource conservation is to be emphasized. Energy and water conservation measures and the collection of recyclable materials are to be integrated into building designs.
- Technology - The latest in technology and communication advances will be incorporated for state of the art business transactions and creating a link to worldwide centers of commerce. Kapolei will be a "smart city."



*Pedestrian-friendly Environment*

## CHAPTER 3: Urban Concept and Themes

### 3.4 Major Themes

## CHAPTER 3: Urban Concept and Themes

### 3.5 Evolution of the City



### 3.5 EVOLUTION OF THE CITY

Densities of specific projects within the City will vary according to market conditions. Based upon the experiences of other cities, initial development densities and infrastructure development are constrained due to start-up economics, lower market demand, and the pioneering required to establish larger scale businesses and service operations in a new locale. As the City matures and development constraints are resolved, densities and the mix of uses are expected to increase. Low density parcels will ideally be redeveloped with higher density developments as market conditions change. This is the pattern witnessed in Honolulu over the past 100 years.

For every development in Kapolei, whether of low or high densities, it is important to adhere to the spirit of the UDP. While developments are anticipated to eventually reach an optimal mature phase, because of market forces and land values, interim phases may not be able to adhere to UDP criteria. For example, structured parking underground or otherwise is encouraged in the UDP. It is recognized that lower density projects cannot support and have no need for parking structures, leaving on-grade parking as the only feasible initial alternative. It is expected, however, as the City matures and as land values and densities increase, that structured parking will become the feasible, preferred alternative. In either case, parking areas need to subscribe to UDP guidelines requiring ample landscaping and façades reflecting kama'āina architecture, contributing to the Hawaiian Garden City concept.

The City of Kapolei  
URBAN DESIGN PLAN

4.0 Supporting Themes and Unifying Elements

## 4.0 SUPPORTING THEMES AND UNIFYING ELEMENTS

The design of the City of Kapolei is also shaped by thematic building blocks which make the City and its districts a functional and attractive urban environment. The thematic blocks also define the City's organizational framework and thus help to ensure its smooth functioning.

Signature details are the final step in the process of defining the City's design character. They illustrate pre-architectural and site design for different City elements, and how to bring life and a distinctive character to the larger parts of the City.

Thematic building blocks for the City of Kapolei are expressed below as block size, connectivity, circulation, views, landscaping, and key unifying elements.

### 4.1 BLOCK SIZE

The 350 feet by 350 feet square blocks in the City of Kapolei are adapted from those in old Honolulu, and provide the framework for an urban level of access and circulation. The relatively small block size encourages people to walk between work, restaurants and shops as their travel route can be flexible, interesting and reasonably direct. Opportunities for spontaneous meetings on the street and interactions result when individuals walk.

<u>CITY</u>	<u>BLOCK SIZE</u>
City of Kapolei	350' x 350'
Downtown Honolulu	275' x 300'
Downtown San Francisco	375' x 375'

### 4.2 CONNECTIVITY

Connectivity describes the directness of links and the density of connections of paths and/or roadways. Increased connectivity decreases travel distances and increases route options allowing more direct travel between destinations, resulting in routes that are more accessible and resilient. A high level of connectivity is more supportive of alternative modes of transport such as biking and walking. Increased connectivity can reduce traffic congestion on major arterials.

City of Kapolei circulation routes achieve a high level of connectivity in several ways. Smaller block sizes are a primary means to achieve a desirable level of connectivity. Other measures include the pedestrian links of Pālailai Mall and Village Walk, the grid street system layout and the minimal use of cul-de-sacs. To maintain connectivity, driveways may be restricted on certain streets. Where driveways are appropriate, they can provide connectivity by being aligned across from roadways (Figure 19).

The City of Kapolei also has multiple connections to the region. There are multiple accesses to the H-1 freeway as part of the Kapolei Interchange Complex. Three major collector roadways link the City of Kapolei to Fort Barrette Road. Kamokila Boulevard, Fort Barrette Road, and future roads in the Mehana District will provide connections to Kalaeloa.



## CHAPTER 4: Supporting Themes and Unifying Elements

4.1 Block Size

4.2 Connectivity

# CHAPTER 4: Supporting Themes and Unifying Elements

## 4.3 Circulation

General Roadway  
Characteristics

Roadway  
Dedication and  
Maintenance

Public  
Transportation

## 4.3 CIRCULATION

### 4.3.1 GENERAL ROADWAY CHARACTERISTICS

The H-1 Freeway links the City of Kapolei with the island-wide community. Kapolei Parkway interconnects the major urban developments throughout the 'Ewa region. Bikeways also are planned for all major roadways to link up with the regional bikeway system. The City's circulation pattern is premised on ample mauka-makai and east-west connectivity.

- Off-Site Improvements

Freeway Interchanges – The City of Kapolei is to be served by the Kapolei Interchange connecting Wakea Street to the H-1 Freeway. Freeway connections are also provided at the Pāilailai and Makakilo Interchanges.

Fort Barrette Road and Kalaeloa Boulevard – Additional turn lanes and through-traffic lanes are to be built when traffic levels warrant them.

- City of Kapolei Roadway Hierarchy

The City of Kapolei Roadway Master Plan, administered by the Campbell Companies and DPP, provides the roadway design standards for the City. The Roadway Master Plan, based on UDP objectives and standards, is updated in conjunction with DPP as necessary. The Mehana District roadway standards are found in the Mehana Roadway Master Plan.

A hierarchy of roadways is provided in the City of Kapolei in recognition of variations in street function and traffic carrying requirements. Street designs that vary in detail from the typical City & County standards are also used to provide roadways that are well landscaped and reflect adjoining land uses.

A map highlighting the City of Kapolei's circulation is found in Figure 5.

### 4.3.2 ROADWAY DEDICATION AND MAINTENANCE

Roadways in the City of Kapolei are to be built by the master and individual project developers or through public-private partnerships. Generally, rights-of-way are dedicated to the City & County of Honolulu once they are completed. It is expected that the City & County and appropriate utility companies will maintain the dedicated streets, utilities, and street lighting.

### 4.3.3 PUBLIC TRANSPORTATION

As of 2007, City & County bus service consists of two regular and three commuter express bus lines. It is expected that regular bus service will eventually be established between the City of Kapolei and all other communities in the region.


A locally-preferred mass transit alignment and stations along Kapolei Parkway throughout the City have been identified and reserved to allow for a future system to link the City of Kapolei, Honolulu and residential communities along the route and to the west of the city.

A rapid transit line is a critical part of a regional transportation system to connect the City to the region and Downtown Honolulu. Transportation alternatives are needed because of the limited capacity of regional roadways. A comprehensive mass transit system is critical to the evolution of the city.

The transit site in the City of Kapolei has been designated in the center of the City. It is a transit hub that integrates buses and park and ride facilities. The transit center has a quarter mile walking distance radius to most of the residential and commercial areas in the City of Kapolei.



**LEGEND**

-  Commercial Arterial
-  Local Commercial/Residential Street
-  Village Walk
-  Bike Lane
-  "Main Street" for Urban Center
-  Locally-preferred Alternative Mass Transit Alignments



Existing Kapolei Transit Service

FIGURE 5

**CIRCULATION**



# CHAPTER 4: Supporting Themes and Unifying Elements

## 4.3 Circulation

Pedestrian and  
Bikeway Systems

## 4.4 Views

### 4.3.4 PEDESTRIAN AND BIKEWAY SYSTEMS

Pedestrian ways and bikeways offer a healthy and environmentally friendly travel alternative thereby reducing automobile use. The conceptual bike plan for the City of Kapolei includes State Bike Plan Hawai'i and Bike O'ahu routes. Bike lanes are on both sides of Kapolei Parkway, Kamokila Boulevard, Kama'aha Avenue, Wakea Street, Fort Barrette Road, and along the Leeward Bikeway (Figure 5). More than 56 miles of regional bikeways are planned (Figure 7) including links to the Pearl Harbor Historic Trail. More information on this subject can be found in the "Kapolei Area Bikeway Plan" and the "Pearl Harbor Historic Trail Master Plan", May 2001. Pedestrians will use Pālailai Mall, Village Walk and roadway sidewalks for access.

### 4.4 VIEWS

There are a variety of views within the City of Kapolei including near and distant natural features and mauka/makai views which have traditionally been important in Hawai'i. The views to be enhanced and defined by the City's development are diagrammed in Figure 6.



*Multiple Modes of Transportation*

- Street Orientation

The orientation of streets within the City of Kapolei is intended to take advantage of near views to local pu'u and distant views to the mountains.

- Mid-Range Views

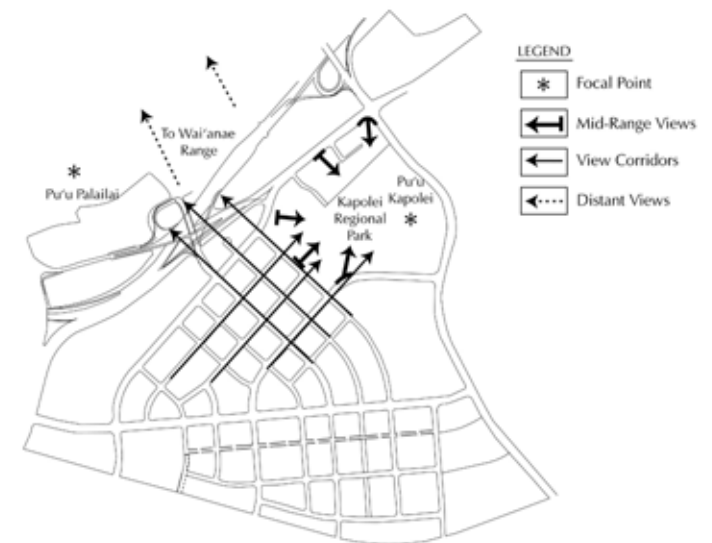
Areas and facilities near Kapolei Regional Park create and take advantage of mid-range views across the park.

- View Corridors

Landscaping of major streets and pedestrian malls is done in a manner which frames and maintains view corridors.

- Pālailai Mall Views

Buildings along Pālailai Mall have views into these view corridors.



**FIGURE 6**

**VIEWS**



FIGURE 7

REGIONAL BIKEWAY SYSTEM

# CHAPTER 4: Supporting Themes and Unifying Elements

## 4.5 Landscaping

### Street Landscaping

## 4.5 LANDSCAPING

Landscaping and streetscape design can heighten visual interest by framing views and providing for a variety of interesting spaces. Streetscapes with extensive use of canopy shade trees reinforce the image of Kapolei as a generously landscaped, green garden city. Variations in the landscape treatment of roadways are intended to identify the street hierarchy and visually unify the districts. The Street Tree Master Plan is shown in Figure 8.

### 4.5.1 STREET LANDSCAPING

#### Objective:

- Provide for a unified and coherent landscape pattern on all streets to create the strong sense of place envisioned for the City of Kapolei.

#### Specific Provisions:

- Trees will be planted in accordance with the City of Kapolei Street Tree Master Plan (Figure 8) and vehicle sight distance safety standards. The specific street trees to be used reflect the scale and hierarchy of the roadways, as well as the various districts of the City.
  - Kapolei Parkway is to have a single row of Monkey Pod trees and groundcover in the medians. The intersections are to have colorful groundcover. Both sides of the roadway are to be planted with groundcover, Hibiscus and a single row of Queen's White Shower trees.
  - Kamokila Boulevard's median is to be planted with True Kamani trees with groundcover. Planting strips along both sides of the roadway are to be planted with Rainbow Shower trees, Asystasia and groundcover.

- Kama'aha Avenue is to have a single row of True Kamani trees and groundcover in the median. The sides of the roadway are to be planted with Rainbow Shower trees. Both sides of the roadway are to be planted with Asystasia and groundcover.

- Fort Barrette Road is to have Queen's White Shower trees on the side adjacent to the City of Kapolei.

- The other roadways in the City are to be planted with the street trees according to the Street Tree Master Plan.
- All landscaped areas are to be provided with a permanent underground automatic irrigation system utilizing non-potable water.
- Except where noted, all sidewalks are to be framed with two rows of trees. The first row is to be in 3-foot wide tree wells adjacent to the curb line. The second row is to be within the adjoining front yard setback area just within the property line. Spacing between trees is to be no more than 30 feet. Both rows are to be offset or staggered.
- Mehana street tree requirements are found in the Mehana Design Guidelines.



Silver Trumpet



White Shower



FIGURE 8

# CHAPTER 4: Supporting Themes and Unifying Elements

## 4.6 Key Unifying Elements

Kapolei Regional  
Park

### 4.6 KEY UNIFYING ELEMENTS

Four key elements link the City's districts together into a distinctive, vibrant city: Kapolei Regional Park, Pālailai Mall, Wai Aniani Way, and the Village Walk (Figure 9).



FIGURE 9

KEY UNIFYING ELEMENTS

### 4.6.1 KAPOLEI REGIONAL PARK



The City of Kapolei's major park occupies a 74-acre site at its northeast end. It functions as the City's visual gateway and a focus for outdoor activities. Kapolei Regional Park is to reinforce both the image and the reality of Kapolei as a place where people can lead an active, healthy lifestyle.

The park provides a diversity of recreational activities. The open field can be used for walking, jogging and playing volleyball, soccer, football, and baseball. Space is also available for picnicking, sunbathing and relaxing.

The Kapolei Regional Park includes the formal Alohilani Garden along Kamokila Boulevard and active recreation areas throughout the remaining area. Facilities for public concerts and gatherings such as an amphitheater and pavilions should be included to help generate both day and evening activity. The makai edges of the park adjacent to the City's "downtown" are designed to ensure a strong relationship between City and park.



Kapolei Regional Park

## 4.6.2 PĀLAILAI MALL



Pālailai Mall will cross Wai Aniani Way extending from its existing mauka end at Campbell Square to its makai end in Mehana. The mall is intended to be an inviting linear park with a path for pedestrians.

The character of Pālailai Mall will be complementary to the adjoining land uses. Its character between Kamokila Boulevard and Haumea Street is shown in the photo. It is comprised primarily of paved areas with seating and some landscaping.

For the portion of Haumea Street to Kama'aha Avenue through the City Center and Commercial Emphasis Mixed Uses districts, the Mall will have more landscaping than the section above. The character will be park-like and businesses will be encouraged to orient outdoor seating areas or cafes onto the Mall. The conceptual plan for three blocks of Pālailai Mall in this area is shown in Figure 10. The history of the area and James Campbell's contributions to the region and accomplishments should be highlighted along this portion (Figure 11).

Between Road M and Kapolei Parkway, which is designated Residential Emphasis Mixed Use, the mall will remain public but assume a more residential character. Seating, other elements of street furniture and vegetation will be consistent with the character of the residential community.

For the character and design for the remainder of Pālailai Mall through the largely residential area of Mehana, see the Mehana Design Guidelines.

Benches, bollards, and trash receptacles may vary along the Mall. Light fixtures, and signage will remain consistent throughout the Mall.



*Pālailai Mall between Campbell Square and Bank of Hawaii*

## CHAPTER 4: Supporting Themes and Unifying Elements

### 4.6 Key Unifying Elements

Pālailai Mall



FIGURE 10

CONCEPTUAL PLAN FOR PĀLAILAI MALL

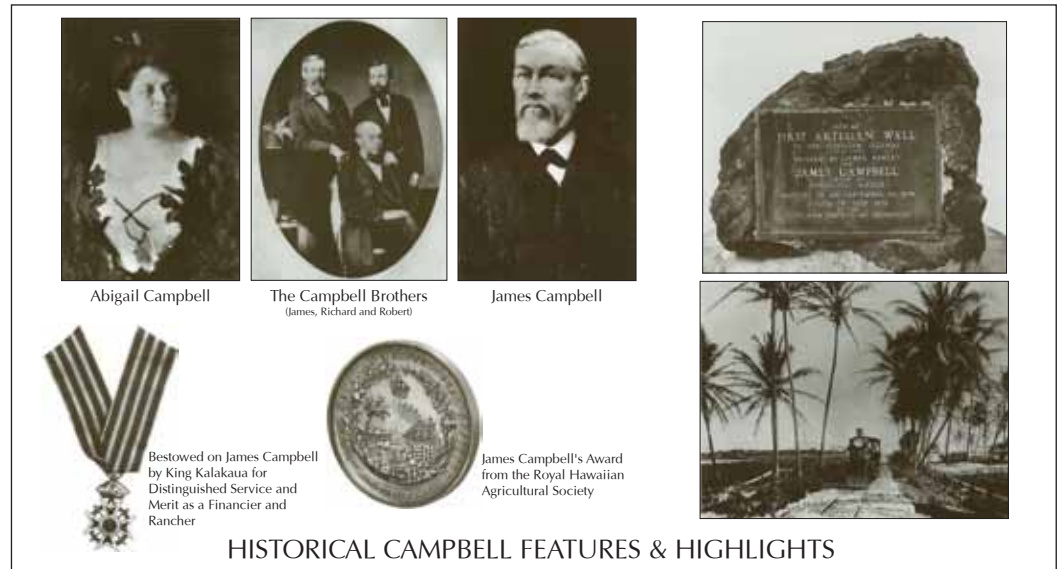


FIGURE 11

SUGGESTED ELEMENTS FOR PĀLAILAI MALL BETWEEN HAUMEA STREET & ROAD M



# CHAPTER 4: Supporting Themes and Unifying Elements

## 4.6 Key Unifying Elements

### Wai Aniani Way



### 4.6.3 WAI ANIANI WAY



Functioning as a "main street" of Kapolei, Wai Aniani Way extends from Kapolei Regional Park through the City Center district to the Civic Center. Its length is roughly the same as Fort Street Mall in Honolulu. Including setbacks, its landscaped corridor is 80 feet wide.

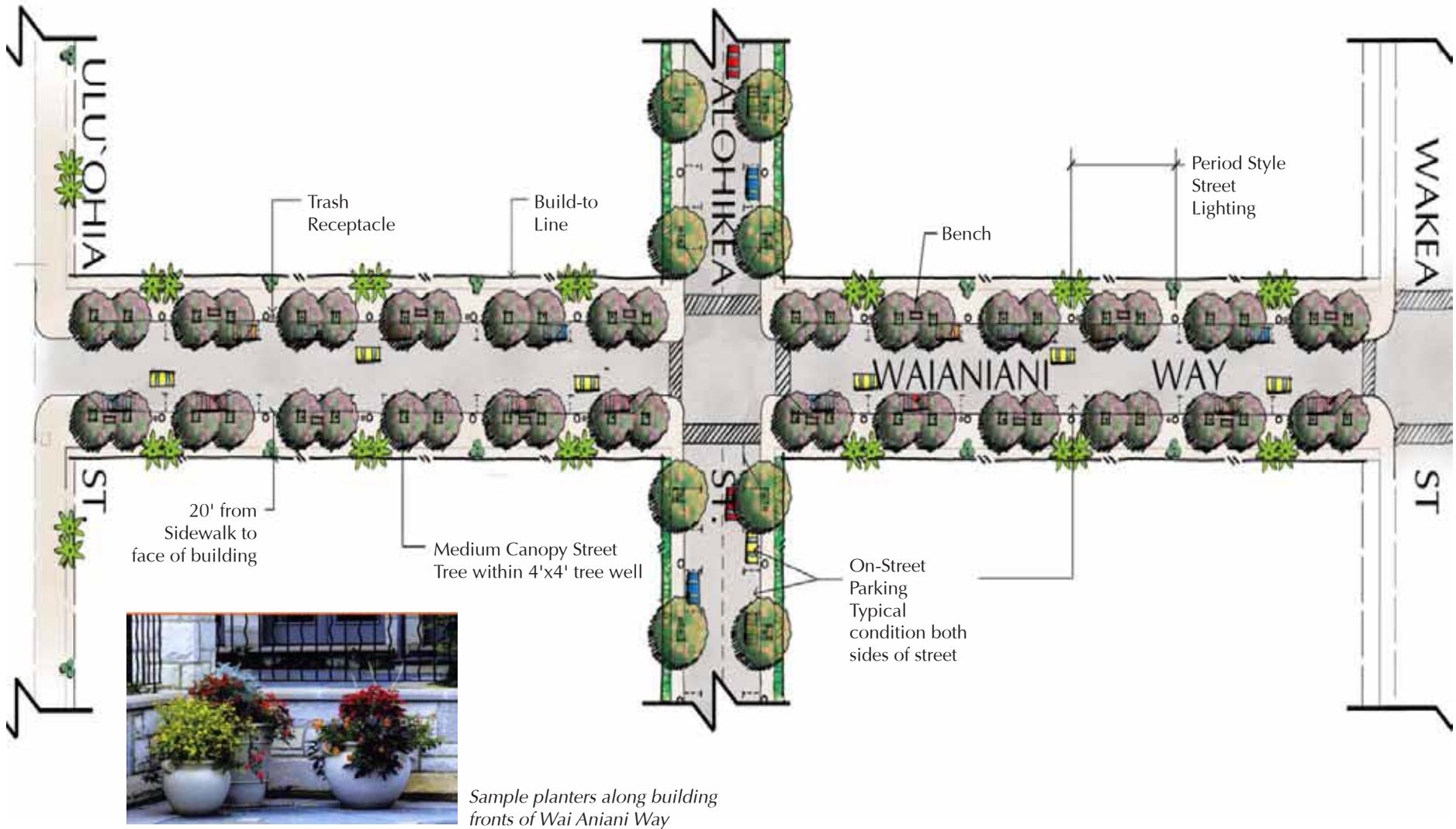
Wai Aniani Way provides a unifying element and pedestrian link that is to be a major contributor toward making the City of Kapolei a distinctive place to live and work. Canopy shade trees, ample landscaping and seating are planned to create a comfortable atmosphere and promote Kapolei as a garden city (Figure 12).

This main street has two-way traffic (one lane in each direction), parking in both directions, and should be designed for low speed travel. Having both pedestrian and vehicular traffic activates Wai Aniani Way as an attraction for City Center retail and commercial activities. Ground floor commercial/retail uses, restaurants, sidewalk cafes, and kiosks provide the diversity and integration of buildings which is required to create a vital activity level.

Vehicular access along Wai Aniani Way is highly discouraged to ensure continuity in the pedestrian streetscape and experience. Subject to LUO, portions of the front yard 5' landscape setback on Wai Aniani Way should be paved to allow planters and street furniture to complement the character of the buildings and to enliven the sidewalk experience without compromising the theme of ample landscaping.



Wai Aniani Way



CONCEPTUAL PLAN FOR WAI ANIANI WAY

# CHAPTER 4: Supporting Themes and Unifying Elements

## 4.6 Key Unifying Elements

Village Walk



### 4.6.4 VILLAGE WALK



Village Walk originates at the Civic Center Judiciary complex, passes along the mauka edge of the Mehana, crosses Pālailai Mall, and terminates at the Mehana school site to the east. It serves as a major pedestrian connector between the Civic Center and Pālailai Mall.

Village Walk is part of a one-way vehicular roadway bisecting the mixed use commercial districts mauka of Mehana with the medium density, mixed use residential areas within Mehana. Village Walk is intended to be a popular, active thoroughfare for residents and an important pedestrian and bicyclist link. While the character of Village Walk may differ by districts, buildings shall be oriented towards Village Walk to enhance the prominence of the corridor and provide opportunities for integrating neighborhood-serving commercial uses (Figure 13).

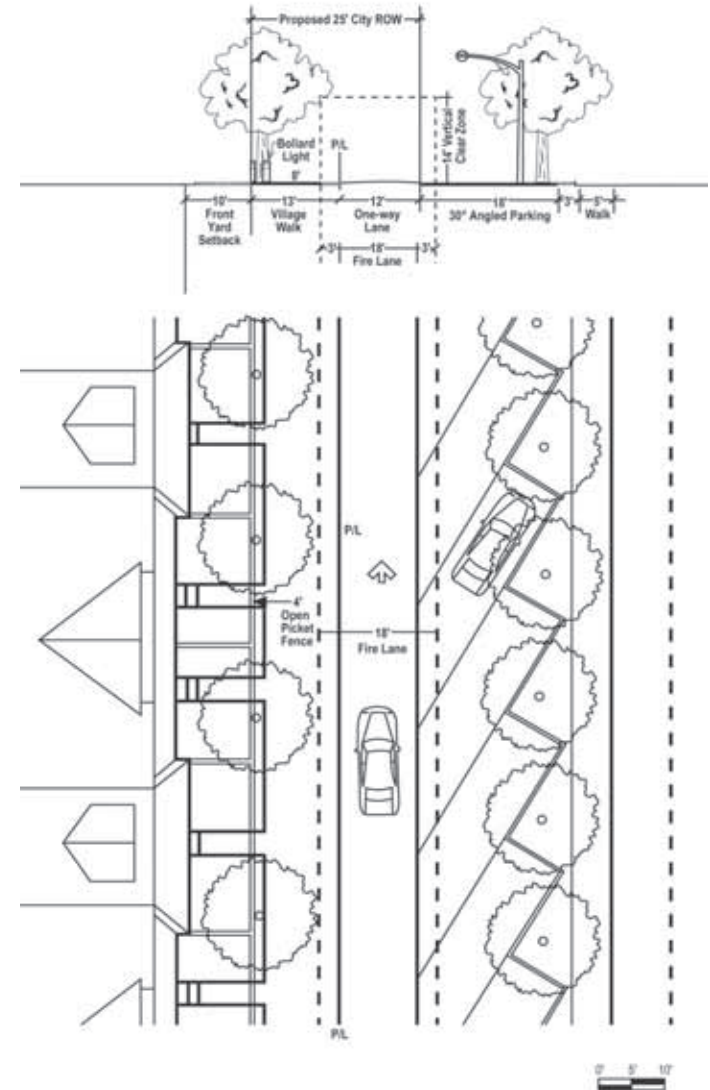


FIGURE 13

VILLAGE WALK  
(City & County of Honolulu  
Ordinance 04-45)

The City of Kapolei  
URBAN DESIGN PLAN

5.0 Districts



## 5.0 DISTRICTS

The physical configuration of the City of Kapolei and how the individual parts will be arranged is its composition.

The City's composition takes the form of seven different districts which are intended to accommodate the full array of business, commercial, residential, cultural, and public uses (Figure 14). Each district is a recognizable area with a distinct image. This image is created from a variety of parts including space, form, building type, detailing, use, and streetscape.

The following development standards and guidelines establish a distinctive character for the City and its individual districts, and provide specific guidance in the design of projects within these districts.



*Configuration of a Project (Bank of Hawaii Building)*

### 5.1 GENERAL GUIDELINES

The following general standards and guidelines apply to all developments within the City of Kapolei. These are followed by more specific standards and guidelines for the seven districts.

#### 5.1.1 OBJECTIVES

- Ensure that individual projects contribute toward the implementation of this UDP.
- Maintain functional and visual continuity in the development of the City.

#### 5.1.2 SPECIFIC PROVISIONS

##### 5.1.2.1 OVERALL DESIGN CHARACTER

- Configuration of Projects

The configuration of buildings and open spaces defined by them should take advantage of public amenities and view opportunities, ensure privacy and safety, and encourage pedestrian activity and social interaction.

- Continuity Within Projects

A project's buildings and landscaped open spaces should maintain a consistent design concept and materials.

- Transition to Adjacent Projects

Project and building design should maintain design continuity with adjacent projects through the use of complementary materials and/or transitions, such as continuity in roof colors. Disruptive visual contrasts should be avoided.

## CHAPTER 5: Districts

### 5.1 General Guidelines

Objectives

Specific Provisions

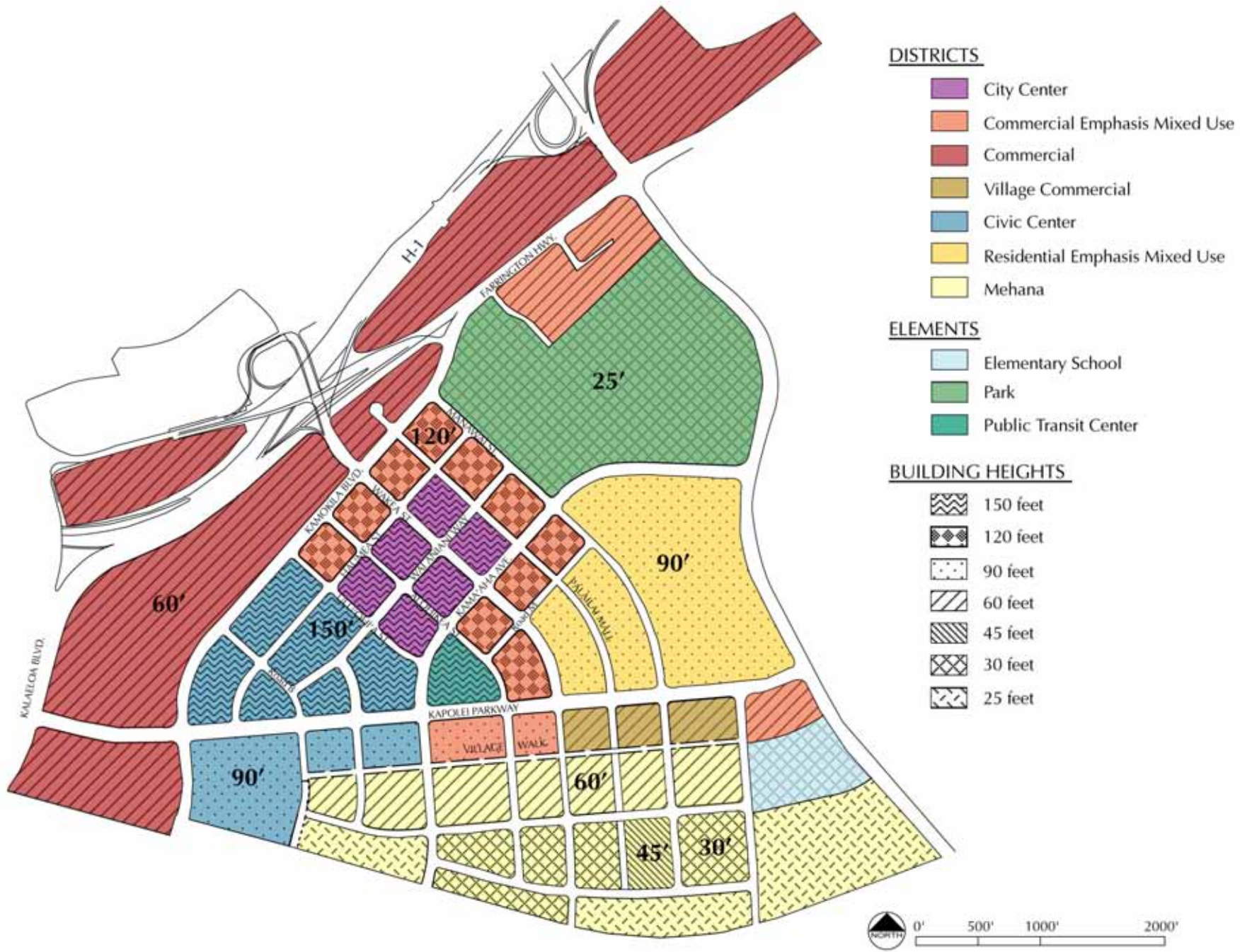


FIGURE 14

DISTRICTS AND BUILDING HEIGHTS

### 5.1.2.2 BUILDING ENVELOPES

- Building Siting

A significant consideration in all developments is their contribution to the street life and distinctive, vibrant public spaces. Building orientation should be toward the street. Developments which turn inward, or which result in leftover or “dead” space, are only allowed when there is no other reasonable alternative. Figure 15 provides guidance for City Center, Mixed Use and Village Center Districts.

- Building Bulk and Massing

Bulk refers to the apparent massiveness of a building compared to its surroundings. Buildings should avoid awkward or overscaled forms where possible. Offsetting building bulk and massing by the articulation of the building form is encouraged.

- Building Heights

The building heights vary by district as shown in Figure 14. All maximum heights are subject to the height restrictions of the underlying zoning. Building height setbacks from adjacent streets as established by the Land Use Ordinance (LUO) are also to be observed (Figure 16). Table 1 shows the allowable building heights.

- Building Setbacks

Successful pedestrian-friendly urban streets are often defined spaces where the buildings come directly to a common setback line. Significant variation from a uniform building setback is discouraged.

- Floor Area Ratios

Floor area ratios for each district shall be in accordance with the LUO and the underlying zoning.

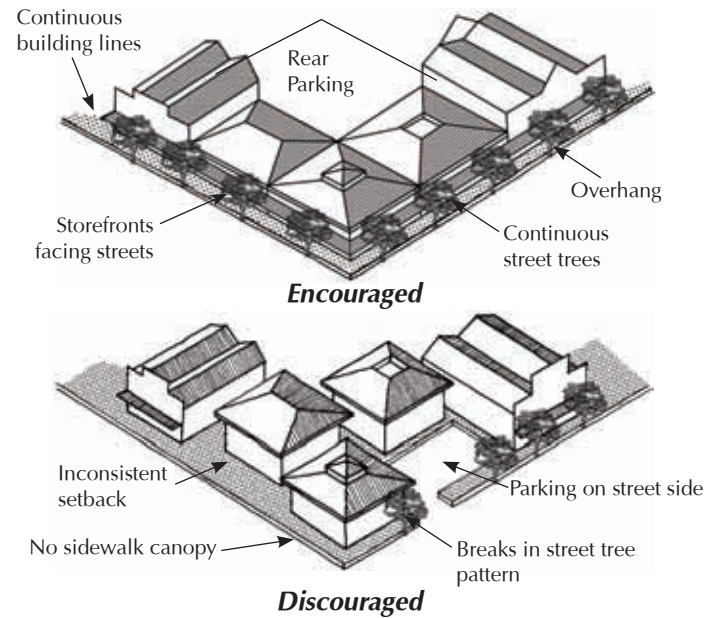


FIGURE 15 GENERAL BUILDING SITING

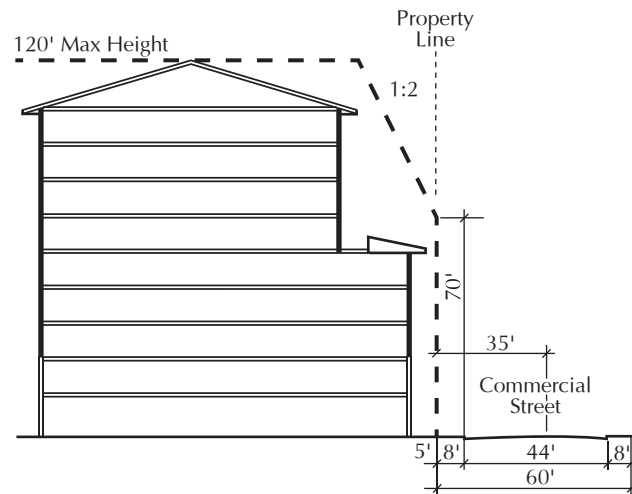


FIGURE 16 EXAMPLE OF STREET SETBACK

## CHAPTER 5: Districts

### 5.1 General Guidelines

#### Specific Provisions

# CHAPTER 5: Districts

## 5.1 General Guidelines

### Specific Provisions

Height	District	Intent
150	City Center	Allows for a high density urban core with the greatest heights at the City's center.
120	Commercial Emphasis Mixed Use	Allows for high density buildings with transitional heights and efficient footprints.
90	Civic Center / Residential Emphasis Mixed Use	Allows for relatively high density buildings throughout the City.
60	Village Commercial / Mehana	Allows for appropriate transitional heights between the City's center and residential areas.
45	Mehana	Allows for medium density residences and supporting commercial.
30/25	Mehana / Parks	Allows for lower density residences and park facilities.

**TABLE 1**

**BUILDING HEIGHT SUMMARY**

### 5.1.2.3 ARCHITECTURAL GUIDELINES

- Architectural Character

The architectural character in the City of Kapolei is to promote a Hawaiian garden city image. Both traditional and modern architectural styles may be used, but should in all cases relate to Hawai'i, in order to create a unique sense of place and a harmonious and inviting environment. Hawai'i's climate and the history of the area should also be taken into consideration.

- Building Materials and Colors

Building materials must be permanent in nature to ensure quality and performance. Use of sustainable and/or recycled building materials is encouraged where available, suitable, cost effective, consistent with existing building codes, and consistent with the UDP. Light pastel colors are encouraged. Excessively bright or garish colors are to be avoided.

- Building Design and Resource Conservation

Energy and water conservation are important considerations in building design, siting, and operations. Air-cooled type air conditioning systems and other energy efficient measures are recommended to conserve water.

Solar water heating and the use of photovoltaics as alternative energy sources are encouraged. Energy saving light fixtures are also encouraged.

- Walls and Fences

Walls and fences for screening should generally be of a material compatible with the building's finishes.





*Off-Set Building Massing (Campbell Building)*

#### 5.1.2.4 SITE DEVELOPMENT GUIDELINES

- Off-Street Vehicular and Bicycle Parking

Each development shall provide sufficient off-street vehicular parking. Parking lots adjacent to streets are highly discouraged.

Conveniently located bicycle racks at a minimum equal to 10% of the number of required parking stalls are also required.

- Loading and Driveways

Loading and unloading areas, service entrances and outdoor storage areas shall be screened and shall not be visible from roadways.

Shared access to loading and parking areas is encouraged and provides less interference with automobile and pedestrian movement.

Driveways should be consolidated as much as possible. Shared driveways are highly encouraged. Consultation with DPP in the early stages of planning driveways is highly encouraged.

- Site Furnishings

Site furnishings are encouraged. Site furniture should be compatible with a project's architectural character and the general character of similar furnishings along public sidewalks and malls.

- Pedestrian Access

Pedestrian facilities and access to all buildings shall be designed to be safe, convenient, comfortable and interesting. This includes providing shade and adequate and attractive shelter and lighting.

- Landscape Character and Quality

Landscaped areas should be used to:

- establish and maintain the image of Kapolei as a green and shaded garden city through the use of xeriscaping
- enhance and complement the character of architecture and sites
- preserve and enhance desirable views
- reinforce circulation patterns

Off-street parking lots shall be landscaped with canopy shade trees and groundcover to reduce direct sunlight and heat. If visible from the public right-of-way, parking lots shall be screened with plant materials, berms, attractive fencing or combinations thereof.

## CHAPTER 5: Districts

### 5.1 General Guidelines

#### Specific Provisions



## CHAPTER 5: Districts

### 5.1 General Guidelines

#### Specific Provisions

- Site Design and Energy Conservation

The following are site and landscape design guidelines for lowering energy use:

- Minimize exterior paved surfaces that are not shaded by trees, awnings, trellises or roofing
- Select and site landscape materials to allow direct air movement towards the buildings and provide shading
- Where possible, screen east and west sides of buildings, especially glazed areas, from low angle morning and afternoon summer sun

- Site Design and Water Conservation

Xeriscaping preserves water and can reduce operational costs. The following xeriscaping principles are encouraged:

- Use lower water demand ("drought tolerant") landscape materials
- Use salt tolerant landscape materials
- Use appropriate irrigation methods (e.g., automated operation) to maximize efficiency
- Where a dual water system is provided, use non-potable water for irrigation
- Improve the soil to absorb and retain water.
- Provide an appropriate level of maintenance
- Consider pervious paving materials to decrease stormwater run-off

- Signage

Signs can be a welcome part of the downtown environment. They provide information and add visual variety to the street. Building signs should be attractive, blend with building façades, and be compatible with signage on adjacent buildings. Refer to "The City of Kapolei Sign Guidelines" for more specific signage standards.



*Large Office Tower in City Center District*

- Exterior Lighting

Light sources should be recessed, screened or shielded to minimize glare and reflection to streets, sidewalks, parking areas, buildings, and adjoining properties. Low intensity light fixtures are preferred.

In surface parking areas, fixtures should be scaled to the pedestrian and the automobile, with light directed downward. Recommended height for fixtures is no more than 20 feet.

Special lighting that enhances landscaping, architectural details, and signage that invites evening pedestrian activity is encouraged.

## 5.2 CITY CENTER DISTRICT



City Center is the high density commercial and office core of the City of Kapolei (Figure 14). Wai Aniani Way serves as its spine and the "main street" of the City. Office towers, with shopping and restaurants at ground level, are ultimately to be the predominant form of development in the City Center.

### 5.2.1 OBJECTIVES

- Create a high density urban environment that is pleasant and active.
- Maintain a comfortable human scale combined with a high degree of vitality at ground level.
- Encourage a high level of pedestrian day and night-time activities, especially along Wai Aniani Way.

### 5.2.2 SPECIFIC PROVISIONS

(See also Section 5.1.2 - Overall Specific Provisions)

#### 5.2.2.1 BUILDING ENVELOPES

- Open Space Bonuses

For each higher density, open space bonuses per the City & County LUO may be appropriate.

- Height

The maximum building height is 150 feet including the required sloping roof form of up to 25 feet (Figure 14). Up to 125 feet may be used as occupied floor space. The required roof form may serve to house elevator and other necessary mechanical equipment. In addition, occupied floors may also occur within the sloping roof form subject to review and approval by the DAB.

- Setbacks

To maintain continuity and a clearly defined edge to public spaces, uniform building setbacks shall generally be observed along streets except where sidewalk cafes and other activities and open spaces are proposed.

#### 5.2.2.2 USES

The predominant uses in this district are to be offices, financial, retail, business support services, personal services, and restaurants. Multi-family dwelling units may be permitted on the upper floors of buildings, subject to obtaining the proper zoning from the City & County.

#### 5.2.2.3 ARCHITECTURAL GUIDELINES

- Architectural Character

The architectural character of buildings should be contemporary and attractive, and incorporate the typical characteristics of classic Hawaiian architecture. Buildings should have tri-partite massing with distinctive bases, bodies and roofs (Figure 17).

Buildings should be pedestrian scaled and incorporate elements of classic Hawaiian architecture such as arcades, gateways, arched forms, and classic windows.

## CHAPTER 5: Districts

### 5.2 City Center District

#### Objectives

#### Specific Provisions

# CHAPTER 5: Districts

## 5.2 City Center District

### Specific Provisions

The building body is that portion between the base and the penthouse and roof. It should be constructed of a combination of masonry, glass and metal.

Roofs should incorporate classic hipped or sloped forms that create an interesting skyline. They should generally be articulated to provide richer forms.

- Building Bulk and Massing

As buildings increase in height, they should attain a more slender appearance. Buildings may vary in height, but there should be consistency in the heights of first floors, awnings, horizontal cornices, etc. along the street.

- Roof Form, Materials and Color

Roof material will be tile. With the concurrence of the DAB, metal may be used only in unique circumstances. Roof colors shall be in the gray/green, blue/gray, and green/brown range.

- Façades

Incorporation of visually interesting details, patterns, texture, fenestration, decoration and/or other scale-referencing features into the design of building façades is encouraged to provide visual interest for the pedestrian.

- Wall Finish and Color

Colors and surfaces should be characterized by being absorptive rather than reflective.

### 5.2.2.4 SITE DEVELOPMENT GUIDELINES

- Building Siting

Buildings shall be sited to integrate with Wai Aniani Way and promote social and commercial activity along the promenade and adjoining streets.

Primary building entries should be oriented to promote pedestrian movement which will support ground level commercial uses.

Vehicular access along Wai Aniani Way is highly discouraged to ensure continuity in the pedestrian streetscape and experience.

Buildings should ultimately cover the bulk of the site. Ground level pedestrian space and landscaping should not cover more than 35 percent of the total site area.

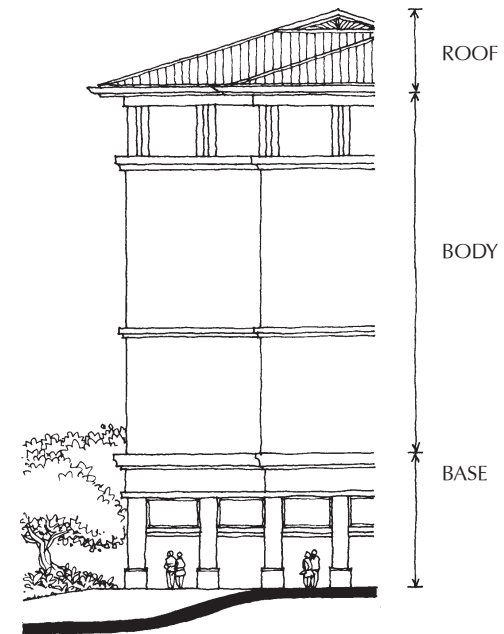


FIGURE 17 CITY CENTER ARCHITECTURAL CHARACTER

- Off-Street Parking

Required off-street parking may be provided either on-site or on adjacent blocks. Joint development of parking structures which provide part or all of the parking required by two or more projects is encouraged. This will require a Joint Use of Parking Facilities Conditional Use Permit (minor) per the City & County LUO.

Entrances to parking structures are to be at least 100 feet from street corners whenever possible. The provision of on-site parking at ground level alongside public sidewalks and/or streets is to be avoided as it does not contribute to pedestrian activity and street vitality. If it cannot be avoided, to reduce the visual impact of parking away from primary viewplanes, parking onsite and ongrade should be bermed with the ultimate goal of creating underground parking when it becomes feasible.

Parking structures must have visually interesting façades, e.g., textured or articulated, and/or be fully landscaped with perimeter planters and vines on each floor above grade.



City Center District

## CHAPTER 5: Districts

### 5.2 City Center District

#### Specific Provisions

## CHAPTER 5: Districts

### 5.3 Civic Center District

#### Objectives

#### Specific Provisions

### 5.3 CIVIC CENTER DISTRICT



The Civic Center is to be a center for governmental, cultural and commercial activities and services in an urban park setting. It is to be composed of open spaces formed by buildings and landscaping which define areas for gatherings, outdoor eating, passive gardens, concerts, etc.

#### 5.3.1 OBJECTIVES

- Create an appropriately landscaped setting for governmental functions which balances built forms and usable landscaped/paved spaces.
- Incorporate characteristics of classic Hawaiian architecture.
- Create a focus for cultural facilities and activities.
- Provide adequate parking, generally underground.
- Provide a transit-centered development with transit station, park and ride area, and day care facilities.

#### 5.3.2 SPECIFIC PROVISIONS

(See also Section 5.1.2 - Overall Specific Provisions)

#### 5.3.2.1 BUILDING ENVELOPES

##### • Height

Maximum building heights in the Civic Center District above Kapolei Parkway shall be 150 feet including the required sloping roof form of up to 25 feet, up to 125 feet may be used as occupied floor space (Figure 14). Below Kapolei Parkway, the maximum building height shall be 90 feet and up to 75 feet may be used for occupied floor space. The required roof form may serve to house elevator and other necessary mechanical equipment. In addition, occupied floors may also occur within the sloping roof form subject to review and approval by the DAB.

##### • Setbacks

Setbacks in the Civic Center District should generally be ample. Except for entry walks and driveways, all setback areas shall be developed as landscaped spaces for sitting, walking, etc. The minimum setback from the property line shall be 15 feet.

#### 5.3.2.2 USES

Uses include governmental offices, cultural facilities, public transportation, day care, and limited supporting commercial uses such as restaurants and convenience shops, to provide convenient services to workers and visitors. Land within the Civic Center District not utilized for civic uses may be utilized for uses consistent with Commercial Emphasis Mixed Use District and the underlying zoning.

A cultural/arts center including theatres and museums is an important part of an urban environment. When growth in the region allows for an adequate level of support and patronage, the appropriate government bodies and private entities should work to establish a cultural/arts center for the City and surrounding region.

### 5.3.2.3 ARCHITECTURAL GUIDELINES

- Architectural Character

The architectural character of the Civic Center District should incorporate characteristics of classic Hawaiian architecture. Buildings should be unified by a consistency of civic scale, refined level of details and high quality of materials.

While individual buildings may differ, all buildings should emphasize compliance to common elements such as horizontal coursing, roof forms, color, and heights. An emphasis should be placed on distinguishing the base, body and roof forms of buildings in order to maintain continuity.

- Building Bulk and Massings

As most buildings within the Civic Center are expected to be free standing, it is especially important that they not appear too monolithic and repetitive in their character. Use of a variety of forms, textures, materials, and shapes is encouraged provided they are consistent in respect to their architectural character.

- Roof Forms, Materials and Color

Building roofs should incorporate classic hip, gable, and mansard forms that create an interesting skyline. They should generally be articulated rather than monolithic in order to provide richer forms. Tile is to be used as a consistent roofing material. Roof colors shall be in the gray/green, blue/gray and brown/green ranges.

- Façades

Visually interesting details, patterns, texture, fenestration, decoration, and/or other scale-giving features are encouraged to provide visual interest. Classic Hawaiian architectural details are encouraged.

Above ground parking façades must be fully landscaped with perimeter planters and vines on each floor above grade. Parking structures must be compatible with surrounding buildings.

- Color and Wall Finish

Colors of exterior wall surfaces should be light earth tones and light natural pastels. Brighter and/or darker colors may be used for accents only.



## CHAPTER 5: Districts

### 5.3 Civic Center District

#### Specific Provisions

## CHAPTER 5: Districts

### 5.3 Civic Center District

#### Specific Provisions

#### 5.3.2.4 SITE DEVELOPMENT GUIDELINES

- Building Siting

Buildings within a block shall generally have a uniform setback along roadways to present a well defined edge along public sidewalks. Breaks in the street façade shall be encouraged to identify entries, plazas, cafes, etc. Where possible, site buildings fronting the sidewalk and observe a uniform setback in order to create a continuous façade. Interior courtyards or areas away from the street are preferred for parking to create a more active pedestrian environment.

- Parking

Sufficient off-street parking to meet requirements of the City & County LUO shall be provided. As a minimum, sufficient off-street parking should be provided on-site to serve the general public. Employee parking may be provided in off-site parking structures.

The development of underground parking structures within the Civic Center is strongly encouraged. The provision of on-site parking at ground level, adjacent to public sidewalks, is to be avoided as it does not contribute to pedestrian activity and street vitality. If it cannot be avoided, to reduce the visual impact of parking away from primary view planes, parking onsite and on-grade should be bermed with the ultimate goal of creating underground parking when it becomes feasible.

- Loading

Shared access to loading and parking areas will be encouraged, provided interference with automobile or pedestrian movement is minimal.

- Site Furnishings

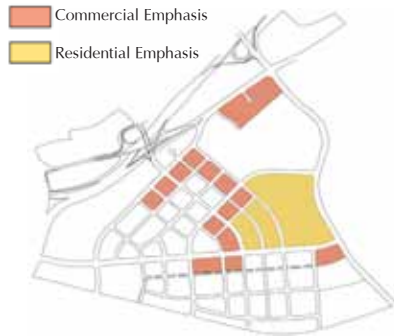
Site furnishings such as benches, lamp standards and receptacles are encouraged and should be compatible with the architectural character of the building.



*City and County of Honolulu Kapolei Hale*



## 5.4 MIXED USE DISTRICTS



The mixed use districts are composed of a mixture of commercial and residential uses of medium density. Commercial development is emphasized in the area adjoining “City Center” opposite the transit center and along Kapolei Parkway as shown in Figure 14.

There are also pockets of mixed use with commercial emphasis mauka of the Regional Park and makai of Kapolei Parkway. Residential use is emphasized makai of Kapolei Regional Park to Kapolei Parkway.

### 5.4.1 OBJECTIVES

- Provide for mutually supportive combinations of convenient and accessible residential, commercial and service facilities.
- Provide variety in urban experiences for pedestrians and to encourage greater social interaction.
- Ensure a suitable residential living environment compatible with neighboring commercial uses.

### 5.4.2 SPECIFIC PROVISIONS

(See also Section 5.1.2 - Overall Specific Provisions.)

#### 5.4.2.1 BUILDING ENVELOPES

- Height

The maximum height of buildings is 120 feet, 90 feet, and 60 feet, including the required roof form (Figure 14 and 18). Up to 105 feet, 75 feet and 45 feet, respectively, may be used for occupied floors. The required roof form may extend an additional 15 feet above the occupied floors, and it may serve to house elevator and other necessary mechanical equipment. In addition, occupied floors may also occur within the sloping roof form subject to review and approval by the DAB.

- Setbacks

To maintain continuity and a clearly defined building edge, uniform building setbacks shall generally be observed along streets. Breaks in the street façade shall be minimized.

#### 5.4.2.2 USES

Ground floor commercial and service uses are to be the predominant type of development in the Commercial Emphasis Mixed Use District. Multi-family housing may also be provided. Dwelling units, if included, shall generally be located on the upper floors of buildings.

In the Residential Emphasis Mixed Use District, multi-family housing development is to dominate. Commercial uses shall be limited to those that conveniently serve residents of the immediate community including meeting facilities.



# CHAPTER 5: Districts

## 5.4 Mixed Use Districts Specific Provisions

### 5.4.2.3 ARCHITECTURAL GUIDELINES

- Architectural Character and Building Elements

The Mixed Use Districts are characterized by a variety of scale, level of detail and materials. While individual buildings might differ significantly, all buildings should emphasize compliance to common elements, such as consistent horizontal lines, roof and arcade heights.

Emphasis should be placed on distinguishing the base, body and roof form of buildings. The use of awnings, arcades, canopied entryways, courtyards, and clear glass at the building base (first floor or first and second floors) is encouraged. The continuity of the building base design should be apparent from building to building. Buildings with commercial office space should be appropriately glazed.

The body of the building can be designed to distinguish one project from another through the use of setbacks, articulation, fenestration, etc. Architectural details, such as balconies and window planters, are encouraged.

- Roof Form, Materials and Color

Required roof forms include mansard and hip and gable. Exceptions may be granted when not visible from major public viewing areas. Tile is to be used as a consistent roofing material. Roof colors shall be in the gray/green, blue/gray and green/brown range.

- Wall Finish and Color

Colors of surfaces should be warm white, light earth tones and light natural pastel. Brighter and/or darker colors may be used for accents only.

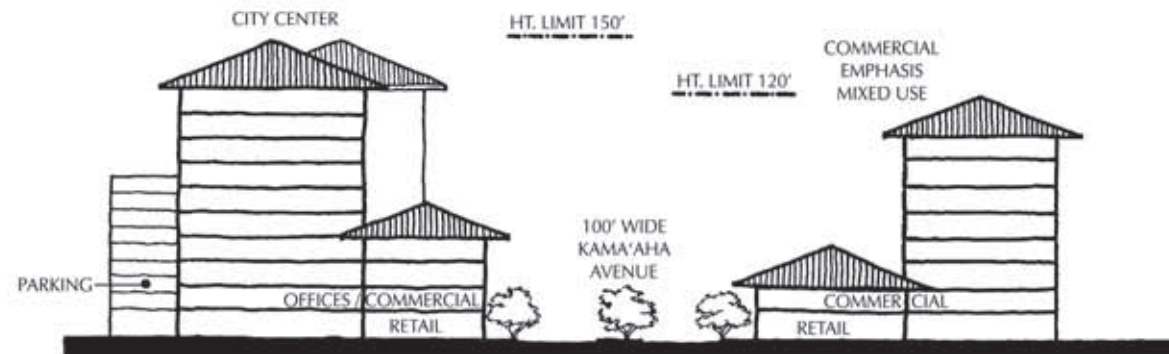


FIGURE 18

BUILDING HEIGHTS AND BUILDING FORMS IN CITY CENTER AND MIXED USE DISTRICTS

#### 5.5.2.4 SITE DEVELOPMENT GUIDELINES

- Building Siting

Buildings within a block shall generally have a uniform setback along roadways to present a continuous facade and well defined edge along public sidewalks. Breaks in the street façade shall be kept to a minimum. Where possible, site buildings directly onto the sidewalk and observe a uniform setback in order to create a continuous façade. Interior courtyards or areas away from the street are preferred for parking to create a more active pedestrian environment.

- Off-Street Parking

Parking should be accommodated away from the street frontage. The landscaping of off-street parking areas shall comply with the off-street parking landscape standards of the City & County LUO.

The provision of on-site parking at ground level adjacent to public sidewalks and streets is to be avoided as it does not contribute to pedestrian activity or street vitality. If it cannot be avoided, on-grade parking shall be screened with adequate berming and landscaping.

- Loading

Shared access to loading and parking areas will be encouraged, provided interference with automobile or pedestrian movement is minimal.

- Site Furnishings

Site furnishings are encouraged and should be compatible with the architectural character of the building and general character of similar furnishings along public sidewalks.



## CHAPTER 5: Districts

### 5.4 Mixed Use Districts

#### Specific Provisions

## CHAPTER 5: Districts

### 5.5 Village Commercial District

#### Objectives

#### Specific Provisions

### 5.5 VILLAGE COMMERCIAL DISTRICT



The Village Commercial area is the local shopping district for residents living in Mehana. It is also conveniently located to serve the day-to-day needs of the people living and working in the Mixed Use District along the mauka side of the Parkway (Figure 14).

The Village Walk is intended as the main street of the Village Commercial District. The District is small in scale and traditional in character and establishes a community identity and sense of place for residents. Mehana residential units along Village Walk will be oriented towards the street to help to enliven the Village Commercial District.

The creation of an active, safe and pleasant pedestrian environment is to be emphasized. To help provide the area with an urban vitality, building entries and display windows are to be oriented to the sidewalk and street, with minimal setbacks and virtually continuous storefronts. Parking is to be accommodated along the street and to the rear of buildings. No parking is allowed on Kapolei Parkway.

#### 5.5.1 OBJECTIVES

- Provide appropriately sized and designed spaces for the establishment of small retail shops, personal service establishments, restaurants, financial institutions, and small offices.

- Create a special place by incorporating many of the elements of the traditional Hawai'i downtown with safe and pleasant walking environments, variation and human-scale detail in architecture, etc.
- Reinforce the linkage provided by Pālailai Mall between Kapolei's City Center and the Mehana District.

#### 5.5.2 SPECIFIC PROVISIONS

(See also Section 5.1.2 - Overall Provisions.)

##### 5.5.2.1 BUILDING ENVELOPES

- Height

The maximum building height is 60 feet (Figure 14). Up to 45 feet may be used for occupied space, with the remaining portion up to 15 feet reserved for an optional roof form. The roof form area above 45 feet shall not be used as occupied space, but it may be used to house elevator and other necessary mechanical equipment.

- Setbacks

Building frontages along streets should be uniformly set back to present a continuous façade. Where permissible, at least a portion of the setback area should be paved and serve as an extension of the public sidewalk.

##### 5.5.2.2 USES

Uses which enhance the desired character of the Village Commercial District include grocery stores, small-scale retail and professional services, restaurants, branch offices of banks, etc. Ground floor uses should generate a lot of foot traffic and thus contribute to the vibrance of the area's environment.



*Typical Architecture Character for Village Commercial District*

### 5.5.2.3 ARCHITECTURAL GUIDELINES

- Architectural Character

The architectural character of Village Commercial buildings should be “traditional” in nature and small scale. Division of the ground floor façades into small scale units is desired to avoid dead wall areas and to offer interest.

- Roof Forms, Materials and Color

Roof forms and roof materials that are compatible with the traditional theme of this area are required. Roof materials may include painted metal. Roof color will be approved on an individual building basis.

- Wall Finish and Color

Principal finish materials should be concrete, stone, terra cotta, plaster, and wood. The use of shiny metal or reflective surfaces, including paints and smooth or plastic-like surfaces are to be avoided. Surface colors should be warm white, earth tones, or natural colors of stone, coral and cast concrete.

- Building Awnings

The inclusion of street side awnings is encouraged. The design of awnings should respect the scale and character of the building and its surroundings, enhance the pedestrian experience, and provide welcome shade.

Awnings shall be placed within the first story of the building and shall generally abut or match the height of awnings on adjacent buildings.



## CHAPTER 5: Districts

### 5.5 Village Commercial District

#### Specific Provisions

## CHAPTER 5: Districts

### 5.5 Village Commercial District

#### Specific Provisions

#### 5.5.2.4 SITE DEVELOPMENT GUIDELINES

- Building Siting

All buildings are to directly front onto the sidewalk, and generally observe a uniform setback in order to create a continuous façade. Permitted building depth is a maximum of 70 feet, with the interior central courtyard area of each block used for parking. Breaks in the street building façade shall be minimized (Figure 19).

- Off-Street Parking

All or a portion of each building's required parking should be accommodated in the center of the block. Adequate and attractive pedestrian access points shall be provided to the block's central parking area. Where all required parking is not provided within the block, the remainder must be accommodated on a nearby block.

The landscaping of off-street parking areas shall comply with the off-street parking landscape standards of the LUO.

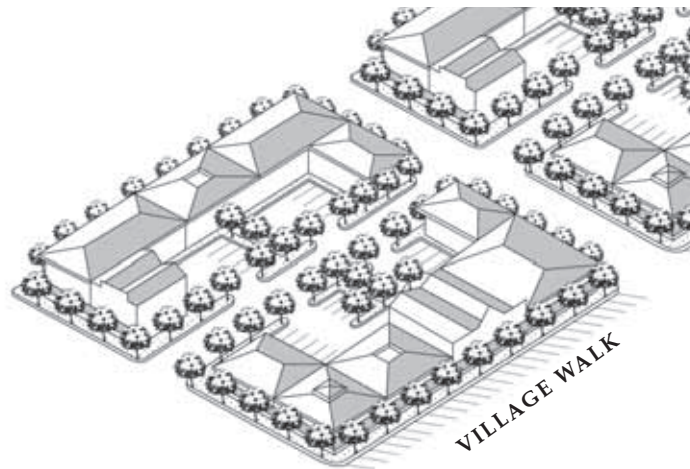


FIGURE 19

VILLAGE COMMERCIAL BUILDING SITING

- Loading

Shared access to loading and parking areas will be encouraged, provided interference with automobile or pedestrian movement is minimal.

- Walls and Fences

Screening walls and fences must be compatible with the building's finishes. Chain link fencing is not allowed.

- Street Lighting

Lighting fixtures will be a traditional style and should contribute to the ambiance of the district.



Typical Lighting Standard

## 5.6 MEHANA DISTRICT



Mehana (“warmth of the sun”) at Kapolei is a 134-acre master-planned mixed use and residential community. It will include approximately 1,150 multi- and single-family residential units within distinct neighborhoods supported by a mixed-use commercial complex, an adjoining elementary school, and parks and recreation facilities.

Residents will be able to work live, work and play within walking distance of the nearby City Center and Village Walk.

### 5.6.1 OBJECTIVES

- Provide multi- and single-family dwellings in an attractive living environment close to the City’s urban center.
- Establish well defined neighborhoods focused around public areas such as pedestrian and recreational amenities and convenient shopping, to encourage interaction among residents.
- Provide ample pedestrian, bicycle and recreational open spaces and minimize the visual intrusiveness of off-street vehicle parking facilities.
- Provide a variety of housing types and prices to accommodate a range of housing options.

- Provide opportunities for neighborhood-serving commercial uses in the mixed use neighborhoods.

### 5.6.2 USES

The allowable land uses in Mehana are governed by the underlying zoning.

### 5.6.3 SPECIFIC PROVISIONS

The controlling document for specific provisions within the Mehana Residential District is the “Mehana at Kapolei Design Guidelines.” The Mehana at Kapolei Guidelines were prepared pursuant to City and County of Honolulu Ordinance 04-46 to establish and maintain a distinctive community and architectural character that complements the rest of the City of Kapolei and creates a high quality living environment.

The Mehana at Kapolei Design Guidelines and projects within Mehana are subject to review and approval by the Campbell Companies, DAB and DPP.



*Mehana Residential District*

## CHAPTER 5: Districts

### 5.6 Mehana District

Objectives

Uses

Specific Provisions

## CHAPTER 5: Districts

### 5.7 Commercial District

Objective

Specific Provisions

### 5.7 COMMERCIAL DISTRICT



The City of Kapolei commercial district includes the Kapolei Shopping Center at the intersection of Farrington Highway and Fort Barrette Road, and other properties along Farrington Highway, Kamokila Boulevard and H-1 Freeway (Figure 14). Structures in this district are likely to consist of one and two-story buildings.

The density of development in this area is likely to be lower than the permitted density because of the high surface area required for parking.

This district is to accommodate commercial uses having the following characteristics and/or requirements:

- Relatively large lot areas.
- Relatively small lot coverages.
- Convenient off-street parking, with most if not all spaces at ground level.

#### 5.7.1 OBJECTIVES

- Provide a contiguous area for a variety of commercial uses.
- Provide relatively large lot areas.



*Typical Architecture in the Commercial District*



## 5.7.2 SPECIFIC PROVISIONS

(See also Section 5.1.2 - Overall Specific Provisions.)

### 5.7.2.1 BUILDING ENVELOPES

- Height

The maximum building height for this district is 60 feet, including a required roof form (Figure 13). The area taken up by the roof form may be used as occupied space.

- Setbacks

The minimum front yard is 10 feet. Side and rear yards shall conform to the standards specified in the City & County LUO for the underlying zoning.

### 5.7.2.2 USES

Uses in the Commercial District are expected to be of relatively low density and/or require large amounts of surface area such as discount or retail outlets, entertainment establishments or home improvement centers. Offices or more intensive specialty-oriented commercial uses may also be located there.

### 5.7.2.3 ARCHITECTURAL GUIDELINES

- Building Bulk and Massing

Structures in this district may tend to be large and bulky, but they should not appear warehouse-like. Buildings should be clustered into complexes where possible, and incorporate smaller and more detailed forms and fenestration to provide human scale.

- Roof Forms, Materials and Color

Mansard, hip or gable roof forms are required. Exceptions may be granted when not visible from major public viewing areas. Tile is to be used as a consistent roofing material. Roof colors shall be in the gray/green, blue/gray, green/brown ranges.

- Wall Finish and Color

Colors of wall surfaces should be warm white, light earth tones and light pastels. Bright and/or dark colors may be used for accents only.



*The Marketplace at Kapolei*

## CHAPTER 5: Districts

### 5.7 Commercial District

#### Specific Provisions

## CHAPTER 5: Districts

### 5.7 Commercial District

#### Specific Provisions

#### 5.7.2.4 SITE DEVELOPMENT GUIDELINES

- Off-Street Parking

The location of parking behind buildings is encouraged. Landscaping standards shall as a minimum meet those provided in the LUO. However, in situations where parking lots are large, in front of buildings, or abutting major streets, etc., more extensive landscape planting may be required.

- Building Siting

Buildings are typically sited along the perimeter of the property while observing the LUO required setbacks.

Free-standing pad buildings situated along Kamokila Boulevard and Farrington Highway should be oriented and designed to reinforce the streetscape. Free-standing buildings in the middle of parking lots should be avoided.

The City of Kapolei  
URBAN DESIGN PLAN

6.0 Project Design Review Procedures

## 6.0 PROJECT DESIGN REVIEW PROCEDURES

### 6.1 PURPOSE

The purpose of the Campbell Companies' design review process is to ensure that all projects in the City of Kapolei conform to and implement the design concept and themes, and the development standards and guidelines set forth in this UDP and thus contribute to the City's overall environmental quality.

### 6.2 APPLICABILITY

Any construction, installation or alteration upon any lot, roadway, utility line, building, or other type of structure; any excavation, filling or change to surface drainage; and any planting or removal of vegetation upon any lot may be undertaken only after review and approval in accordance with these procedures.

Nothing herein removes or otherwise affects the responsibility of each project developer for satisfying all applicable laws, codes and ordinances, and for obtaining all permits and approvals required by law. Should a conflict arise between any of the UDP standards and any law, code or ordinance, or any permit or approval condition, the stricter provision will prevail.


### 6.3 PARTICIPANTS

The Campbell Companies and the DPP retain design review and approval authority over all developments in the City of Kapolei.

In order to assist in the review of projects and interpretation of the provisions of this UDP, Campbell Companies has established a DAB comprised of professionals in various fields including but not limited to architecture, planning, landscape architecture and engineering.

The Campbell Companies with the DAB will pre-screen projects as a part of the land sale process to minimize blatant UDP inconsistencies and poor design. The Campbell Companies will also encourage applicants to seek DPP review early in the project development process and to invite a Campbell representative familiar with the DAB process to the DPP meeting.

The letters and plans submitted by applicants for DAB review will be forwarded to DPP, as will DAB meeting notes and approval status letters. The applicant should include the DAB approval letter to DPP with building permit application(s).



## CHAPTER 6: Project Design Review Procedures

6.1 Purpose

6.2 Applicability

6.3 Participants

# CHAPTER 6: Project Design Review Procedures

## 6.4 General Review Standards

## 6.5 Minor and Major Projects

## 6.6 Review Procedures - Minor Projects

### 6.4 GENERAL REVIEW STANDARDS

In reviewing plans and specifications, the DAB and Campbell Companies will be concerned with both the overall design concept and the details of the design. General concerns will include whether the proposed project:

1. Contributes to the implementation of the urban concept and key design elements, and is consistent with the development standards and guidelines of the UDP.
2. Is compatible and in harmony with existing and approved structures and other improvements in the area in terms of exterior design, quality and type of materials and workmanship, and relationship to topography and ground elevation.
3. Will not, because of its design, unreasonably interfere with the light and air or view of adjoining lots.
4. Promotes resource conservation through energy efficiency, water conservation, recycling and other environmentally sustainable practices.
5. In other respects constitutes a suitable and adequate development of the property.

Plans found to be inconsistent with the urban concept, themes or city structure established in this UDP shall be rejected. Major variations from the development standards and guidelines contained in this document shall also be rejected. Determinations of consistency, and of whether a project constitutes a "suitable and adequate" development, shall be at the sole discretion of Campbell Companies.

### 6.5 MINOR AND MAJOR PROJECTS

Separate processes are established for the review of "Minor Projects" and "Major Projects." Examples of minor projects include providing small structures, changing a building's

color, adding awnings at ground level, or replacing plant material. The determination of which process is to be applied to each project rests initially with Campbell Companies, but is open to appeal through the DAB.

### 6.6 REVIEW PROCEDURES – MINOR PROJECTS

Upon notice of a proposed action, Campbell Companies will determine whether the project will qualify for minor project processing. If so qualified, Campbell Companies may consult with one or more of the members of the DAB. The entire DAB would not be convened.

Campbell Companies and consulted DAB members will review the project at its various stages. The content and number of copies of required submittals will be as directed by Campbell Companies. Phases of the review will generally be as follows:

1. Schematic Submittal - At the initiation of the project, a verbal and graphic submission should be made which outlines the action, describes its major characteristics, and briefly assesses its impacts on any existing or approved site improvements and adjacent properties.
2. Design Development Submittal - After approval of the Schematic Phase, drawings addressing schematic design comments should be submitted for design development review. Emphasis should be given to relationships (setbacks, colors, materials, etc.) to adjacent properties and existing buildings.
3. Final Submittal - Should approval be given at the Design Development Phase, final drawings and other documents should be submitted for final approval.

Campbell Companies and consulted DAB members will, whenever possible, complete all phases of the review within 30 days exclusive of the applicant's preparation time.

## 6.7 REVIEW PROCEDURES – MAJOR PROJECTS

For actions determined to be major projects, the review process described below and illustrated in Figure 19 will be followed:

1. Pre-design Meeting - This meeting may include the following participants: the applicant, the project architect, representative(s) of Campbell Companies, and a representative of the DAB.

The purpose of the meeting is to introduce the applicant and the project architect to the design and environmental goals for City of Kapolei, and to provide a context for further work and reviews. The applicability to the project of the overall design framework and the specific development standards and guidelines established in this UDP will be discussed. In particular, information regarding infrastructure and elements such as pedestrian ways, bikeways, roadways, and landscaping will be clarified. Information regarding the character of the region will also be provided.

2. Schematic Design Meeting - This meeting is to include the following participants: the applicant, the project architect and other appropriate consultants, representatives of Campbell Companies and the DAB.

At least seven days prior to the meeting, the applicant is to submit 12 half-sized schematic plans to Campbell Companies for distribution to the DAB. The schematic plans should include sufficient information to show how the proposed design satisfies the parameters established at the Pre-design Meeting and the development standards and guidelines of this UDP.

The review will include the following:

- a. Site plan considerations including traffic flow, pedestrian ways, parking, service, etc. The site plan should show relationships to adjacent properties.
- b. Overall building massing considering view planes, heights, setbacks, etc. All major sections and elevations should be indicated.
- c. Building characteristics including architectural style, volumetric forms, building materials, colors, etc. Perspective drawings and/or models are encouraged.
- d. Landscape plans showing concept, general planting characteristics, etc.
- e. Basic environmental effects (i.e., sunlight and shade exposure, wind velocity, drainage), especially on adjacent properties.
- f. Energy and water conservation methods.
- g. Provisions for recycling and use of recycled materials.

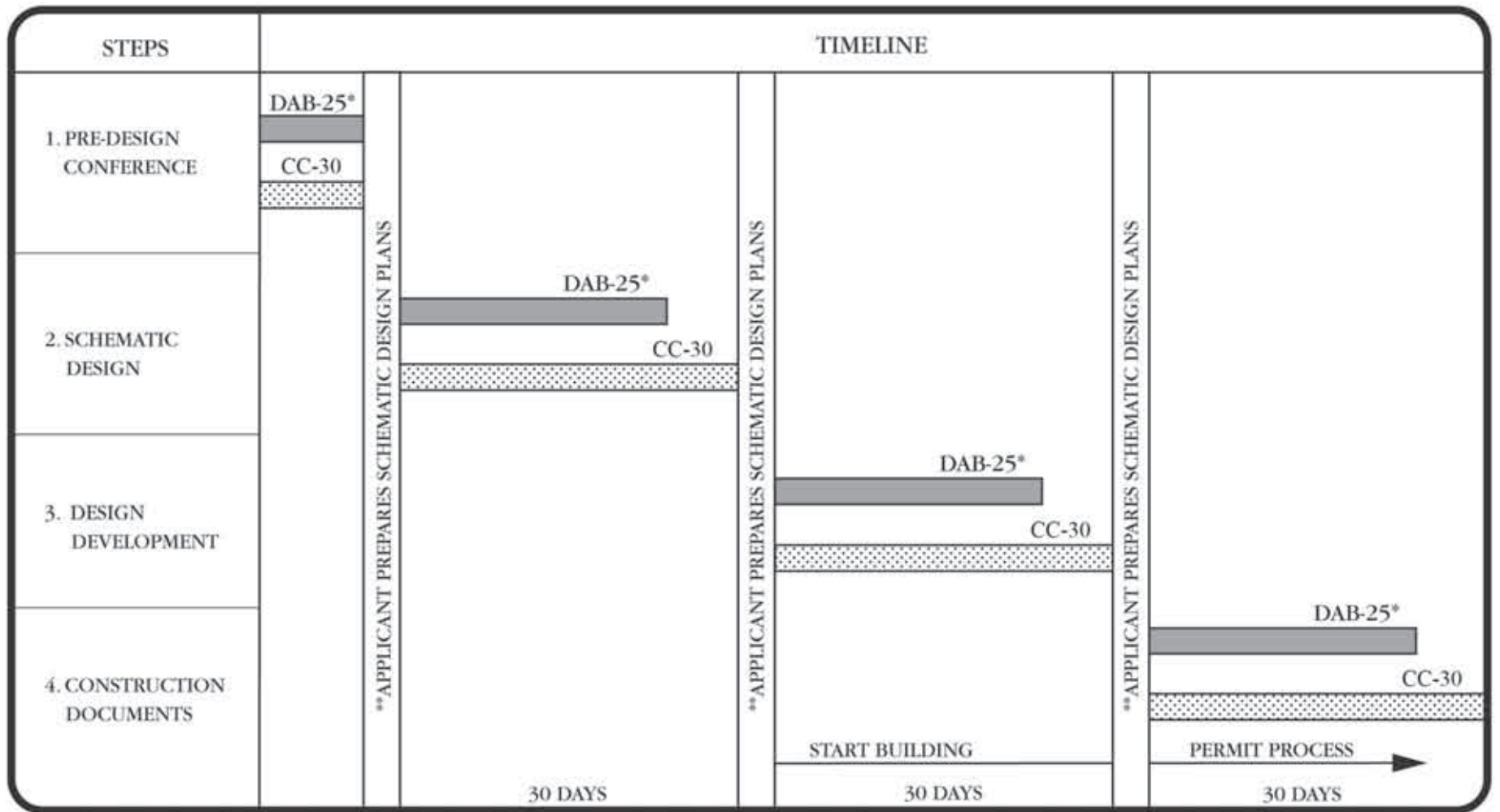
Applicants should consult with DPP during the early stages of this phase of the review process to ensure consistency between DAB and DPP reviews and compliance with the UDP and City and County regulations.

Whenever possible, recommendations arising from the DAB review will be forwarded to the applicant within thirty (30) days of the meeting. Other meetings in the schematic stage may be necessary if the design is not initially approved. The review period may be extended for up to thirty (30) additional days to review plans for large projects or projects which require more study.



## CHAPTER 6: Project Design Review Procedures

### 6.7 Review Procedures - Major Projects



DAB - DESIGN ADVISORY BOARD  
 CC - JAMES CAMPBELL COMPANIES, LLC

\* IF NOT APPROVED, REVISE PER DESIGN REVIEW COMMENTS.

\*\* THE APPLICANT SHALL BE RESPONSIBLE FOR MEETING ALL PERTINENT REQUIREMENTS COVERING DEVELOPMENT OF THE SUBJECT SITE

FIGURE 20

MAJOR PROJECTS DESIGN REVIEW PROCEDURE DIAGRAM

3. Design Development Meeting - This meeting is to include the following participants: the applicant, the project architect, any other appropriate consultants, representatives of Campbell Companies, and the DAB.

At least seven days prior to the meeting, the applicant is to submit 12 half-sized design development plans and outline specifications to Campbell Companies for distribution to the DAB.

The information to be provided on the design development plans includes the following:

- a. Site plan drawings shall, at a minimum, include the following information: all building locations and size, number of stories, roof overhangs and setbacks; locations of roads and walks; location and size of parking areas and/or parking structures, and a description of basic parking requirements for the project; location and size of loading areas; locations, size and intended use of any recreational facilities, courtyards, water features, etc. Ground elevations with existing and finished grades, drainage, earth work, utility lines, etc. should be indicated. Special attention should be given to relationships to adjacent properties. Also, energy conserving methods should be identified.
- b. Review of conceptual floor plan drawings at a scale of at least 1/8" = 1'-0" for all building types. Special attention will be directed at public spaces such as entry lobbies, courtyards, restaurants, etc.
- c. Review of elevation drawings. Inclusion of perspective drawings and a model is encouraged. Special attention will be given to roof forms and materials, balcony and arcade treatments, elevator penthouses, mechanical stacks, trellises, etc. Proposed colors will be a major concern.

- d. Review of sections of the buildings and site. Attention will be given to any major changes in ground elevations in regard to drainage, views and adjacent properties.
- e. Review of landscape drawings. These drawings should show the location, type, size, and quantity of all plant materials, walks, landscape lighting, signs, paved areas, rock work, etc.

The design development review will be completed within 30 days, and a report forwarded to the applicant containing the recommendations and requirements arising from the review and meeting. The review period may be extended for up to 30 additional days to review plans for large projects or projects which are deemed to require more study.

Approval will depend on the extent to which the proposed design satisfies the objectives, standards and criteria established in previous reviews, as well as those identified in this UDP. Other meetings in the design development stage may be held if the design is not initially approved. In no case should the applicant proceed with construction documents prior to design development approval.

4. Construction Documents Review\* - Construction documents will be checked for compliance to design review comments. Two sets of half-sized construction drawings and specifications should be submitted to Campbell Companies. Approval of the documents or a report listing required modifications will be forwarded to the applicant within thirty (30) days of their receipt. The review period may be extended for up to 30 additional days to review plans for large projects or projects which are deemed to require more study. Drawings should be accompanied by a CD containing the overall site plan, landscape plan and roof plan of the project.



## CHAPTER 6: Project Design Review Procedures

### 6.7 Review Procedures - Major Projects



# CHAPTER 6: Project Design Review Procedures

6.8 Construction  
Review and  
Approval

6.9 Variances

6.10 Fees

Approval of construction documents by the DAB and Campbell Companies does not constitute authorization to proceed with the project. Compliance with applicable codes, laws, ordinances, and government agency conditions of approval is the responsibility of the applicant.

*\*Construction Document Review is not required for Mehana plans. Schematic Design and Design Development submissions with sample materials are the essential review points.*

## 6.8 CONSTRUCTION REVIEW AND APPROVAL

1. Duration of Final Approval - Any approval provided shall be effective for a period of 12 months and shall be deemed revoked if the approved construction, reconstruction, refinishing, alteration, or other work approved thereby has not begun within the 12-month period. Campbell Companies may upon request extend the 12-month approval period.

If approval lapses hereunder, the owner or lessee shall be required to resubmit the final plans and specifications for approval. The DAB and Campbell Companies shall not be bound by any previous decision in reviewing such plans and specifications, but shall either approve or disapprove the same in writing within thirty (30) days after such resubmission.

2. As-built Plans - Upon completion of construction, a complete set of as-built plans and specifications for infrastructure improvements will be provided to Campbell Companies.

## 6.9 VARIANCES

Variances from the development standards and guidelines of the UDP may be approved if they are found to be consistent with the goals for the City of Kapolei, and if they meet high development standards.

## 6.10 FEES

Professional fees and the expenses incurred by the DAB members in reviewing and approving plans will be paid by the applicant at each stage of the review process.

The City of Kapolei  
URBAN DESIGN PLAN

7.0 Amendment Procedures

## 7.0 AMENDMENT PROCEDURES

### 7.1 PURPOSE

To respond to changing conditions, modifications to the provisions of this UDP may from time to time be appropriate. The purpose of these amendment procedures is to provide a process for modifications when requested by Campbell Companies, DPP, landowners or prospective developers.

### 7.2 APPLICABILITY


Only applications affecting provisions related to key unifying elements, districts, supporting themes, and the development standards and guidelines will be considered for processing.

As previously noted, the provisions of this UDP are general in nature and are not intended to be comprehensive in their coverage or exhaustive in detail. Amendments will generally be necessary only in instances where a project's design concept or basic design details substantially diverge from the provisions of these documents. Minor variations will be considered through a variance procedure, as described in Section 6.9.

### 7.3 APPLICATION REQUIREMENTS

Applications for amendments must include the following information and analyses:

1. Existing provisions of this UDP for which amendments are being requested.
2. Amendments (i.e., deletions, modifications and/or new provisions) which are proposed to establish project consistency.
3. The basis or rationale for the proposed amendments.



## CHAPTER 7: Amendment Procedures

7.1 Purpose

7.2 Applicability

7.3 Application  
Requirements

## CHAPTER 7: Amendment Procedures

### 7.4 Review Procedures - Districts, Key Unifying Elements, and Supporting Themes

### 7.5 Review Procedures - Development Standards and Guidelines

### 7.6 Approval

#### 7.4 REVIEW PROCEDURES: DISTRICTS, KEY UNIFYING ELEMENTS, AND SUPPORTING THEMES

Applications to amend provisions related to districts, key unifying elements, and supporting themes in this UDP will be processed in accordance with the following procedures:

1. Pre-application Meeting - This meeting will include the following participants: the applicant, the project architect, a representative(s) of Campbell Companies, and a representative of the DAB.

The purpose of the meeting is to briefly review the nature and purpose of the proposed amendment(s). To the extent possible, areas of particular concern which will need to be addressed in the application will be identified.

2. Application Review - Applications must be approved by both the DAB and Campbell Companies. Written applications (12 copies) containing all required information must be submitted to Campbell Companies for distribution to the DAB at least seven (7) days prior to the DAB meeting at which a proposed amendment is scheduled for review.

Whenever possible, the review will be completed and action taken within thirty (30) days of the DAB meeting. The DAB or Campbell Companies may extend its review period for up to thirty (30) additional days.

#### 7.5 REVIEW PROCEDURES: DISTRICT SPECIFIC GUIDELINES

Applications for amendments affecting only the District Specific Guidelines will be reviewed by Campbell Companies in consultation with one or more members of the DAB.

A written application will need to be submitted prior to any project reviews. Its preparation should be preceded by a discussion with the appropriate Campbell Companies representative as to its content.

In general, review of an application through telephone conversations and written correspondence will be satisfactory. Campbell Companies and consulted DAB member(s) will, whenever possible, complete their review within fifteen (15) days of submittal. The review period may be extended for up to fifteen (15) additional days.

#### 7.6 APPROVAL

All applications for amendments to this UDP must be approved by both Campbell Companies and the City Council by resolution.

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